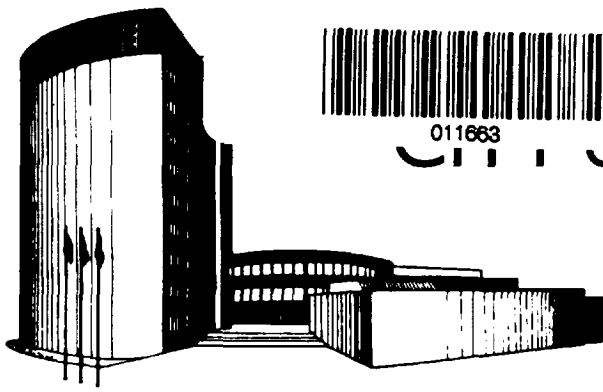


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VIRGINIA VALENTINE



CITY OF LAS VEGAS

November 5, 1998

Mr. Donald R. Cavan
 Grand Teton, Limited Liability Company
 444 East Warm Springs Road, #120
 Las Vegas, Nevada 89119

RE: Z-16-98 - REZONING (Related to GPA-6-98)

Dear Mr. Cavan:

The City Council at a regular meeting held October 12, 1998 APPROVED the request for a Rezoning on property located on the southwest corner of Iron Mountain Road and Decatur Boulevard, From: R-E (Residence Estates) To: R-PD4 (Residential Planned Development - 4 Units Per Acre), PROPOSED USE: 2,336 LOT MASTER PLANNED RESIDENTIAL COMMUNITY, Size: 534.00 Acres. The Notice of Final Action was filed with the Las Vegas City Clerk on October 13, 1998. This approval is subject to:

1. The application shall be amended to R-PD2, R-PD3 and R-PD5 per the Conceptual Site Plan for Iron Mountain Ranch submitted as part of the revised application for Rezoning. This amendment does not include the 80 acres located on the east side of Jones Boulevard between Iron Mountain Road and Horse Drive which is shown for informational purposes only.
2. The project shall consist of a maximum of 1,886 units with a 5% fluctuation thereof.
3. A deviation from City Streetlighting Standards shall be allowed on Jones Boulevard and Bradley Road to allow the streetlights to be the same style fixtures as used in Summerlin and to allow the wattage to be reduced to 100 Watts; however, the lighting shall meet City Standard spacing requirements as required by the Department of Public Works. All other perimeter streets shall meet City Standard lighting requirements.
4. Public sidewalks must be constructed on both sides of collector and arterial public streets adjacent to and within this site as required by the Department of Public Works.



5. A deviation from City Streetlighting Standards shall be allowed on all interior residential streets to allow Summerlin style streetlights at all intersecting streets and all locations where the streets change direction; private streetlighting may be allowed on interior residential public streets to be located outside of the public street right-of-way subject to the approval of the layout by the City Planning Engineer and such streetlights shall be maintained by a Homeowner's Association as required by the Department of Public Works.
6. Contribute the developers proportionate share, not to exceed \$8,600, towards the resolution of the Decatur Boulevard alignment within 30 days of determination of the actual purchase cost of the land from the Bureau of Land Management as required by the Department of Public Works.
7. The Guidelines shall be amended to require single-story homes on the four lots located along the north boundary of Planning Area 10.
8. The Guidelines shall be amended to require a minimum 10 foot side yard setback for the following properties: 1) side yards immediately adjacent to the south side of Grand Teton Drive between Jones Boulevard and Bradley Road; 2) side yards immediately adjacent to the north side of Horse Drive between Jones Boulevard and Bradley Road.
9. The Guidelines shall be amended to require minimum square footage restrictions of 2,300 habitable square feet on all 20,000 square foot lots and 2,500 habitable square feet on all 30,000 square foot lots. The applicant shall incorporate this condition into its Conditions, Covenants and Restrictions established for the project.
10. The Guidelines shall be amended to require that decorative block perimeter wall construction be completed adjacent to perimeter streets prior to issuance of building permits for each construction phase.
11. Section 4.4.2 of the Guidelines sets forth the ability for the applicant to construct City park facilities in lieu of providing a portion of the required open space for the project. The Guidelines shall be amended to require that all such facilities be equestrian related. Said equestrian facilities shall be in substantial conformance with those previously prepared by Cella Barr Associates titled Final Master Plan 40 Acre Park dated May 12, 1993 subject to approval by the City of Las Vegas. The Guidelines shall also be amended to require that park construction commence when fifty percent of the master plan building permits have been issued.
12. The Guidelines shall be amended to require rural street lighting standards to be constructed within the master plan including Summerlin style street light poles and fixtures subject to the approval of the City of Las Vegas.

13. Provide a 20 foot enhanced landscape setback which includes a 5 foot meandering sidewalk in the following locations: a) east side of Jones Boulevard between Whispering Sands Drive and Iron Mountain Road; b) north and south sides of Horse Drive between Jones Boulevard and Thom Boulevard; c) east side of Thom Boulevard between Grand Teton Drive and Brent Lane; and d) north side of Brent Lane between Bradley Road and Thom Boulevard.
14. Planning Area 23 and Planning Area 25 on the Site Plan have been shown for planning purposes only. Approval of the current application will not result in approval of these planning areas. Separate annexation, general plan amendment and zoning change applications will be required for these planning areas. The applicant agrees that future applications for these planning areas will conform with the Site Plan currently on file. The applicant agrees that the density calculations for the 7,000 square foot lots and 10,000 square foot lots in Planning Area 23 and Planning Area 25, respectively, shall exclude the 20,000 square foot buffer lots adjacent to Jones Boulevard.
15. The Guidelines shall be amended to require that the Sheep Mountain and Tule Springs Homeowner Associations be notified prior to any and all public hearings for the master plan.
16. All minimum lot sizes that are reflected on the Site Plan summary are net of public right-of-way.
17. The Guidelines shall be amended to specify that all $\frac{3}{4}$ acre lots are a minimum of 30,000 square feet (net of public right-of-way).
18. The Guidelines shall be amended to require that all minimum 20,000 and 30,000 square foot lots shall have a minimum frontage width of 100 lineal feet.
19. Planning Area 1 shall consist of nine (9) minimum 30,000 square foot and six (6) minimum 20,000 square foot lots. The 30,000 square foot lots are required to front Jones Boulevard and the 20,000 square foot lots are required to front Whispering Sands Drive.
20. Planning Area 8 shall include ten (10) minimum 30,000 square foot lots. The 30,000 square foot lots are required to front Bradley Road.
21. Planning Area 15 shall consist of nine (9) minimum 30,000 square foot lots. The Site Plan summary incorrectly lists these as minimum 20,000 square foot lots and shall be amended.
22. Section 2.3.1 of the Guidelines shall allow on-site equestrian uses on all minimum 30,000 square foot lots within Planning Areas 1, 8 and 15.

23. Section 2.3.2. of the Guidelines shall be amended to allow on-site equestrian uses on all minimum 20,000 square foot lots within Planning Areas 1, 7, 8, 10, 12, 16 and 17.
24. Sections 2.4 and 8.3.1 of the Guidelines, which currently allow the transfer of up to 10% of the units (lots) from one planning area/village to another planning area/village, shall be amended from up to 10% maximum to up to 5% maximum. Under no circumstances does this allow the maximum dwelling units per acre as listed in the Site Plan summary to be exceeded for any individual planning area.
25. The rear yard setback standard for the 20,000 square foot lots shall be amended from 25 feet to 35 feet by amending Section 7.16 of the Guidelines. The asterisk note allows a reduced rear yard setback for the 7,000, 20,000 and 30,000 square foot lots on knuckles and cul-de-sacs. The asterisk note shall be amended to disallow all reduced rear yard setbacks with the exception of the 20,000 square foot lots. A reduced rear yard setback of 30 feet minimum will be allowed on the 20,000 square foot lots that front onto knuckles and cul-de-sacs.
26. Exhibit 7.12 Plant Material List of the Guidelines shall be amended to include Desert/Mexican Bird of Paradise with thorns (common name).
27. An amended Site Plan and Guidelines shall be submitted within two weeks after City Council approval with all of these clarifications and minor amendments reflected.
28. The applicant agrees that if the applicant sells, transfers or otherwise disposes of any of the property that is subject to this application, then such transferees by contract with the applicant will be made aware of all conditions as negotiated with the area homeowners and any transferee agrees to abide by the same.
29. The Master Plan booklet of Guidelines and Rules for this development shall be incorporated into the record and provided to Planning & Development staff.
30. Expedited improvement of Decatur Boulevard and the Beltway.
31. Expedited installation of paving improvements in conjunction with the adjacent Lynnbrook project along Decatur Boulevard from this project to the Beltway and Ann Road.

32. In the event Champion Homes purchases these parcels, Planning & Development shall initiate actions to keep the two out parcels along Jones abutting Grand Teton and Horse Drive as Desert Rural, reclassify the parcel bordered by Grand Teton and Decatur from Medium Low to Low and classify all parcels between Racel and Grand Teton/Bradley Road and Thom Boulevard as Desert Rural.
33. If the Clark County School District acquires a site located within the 40 acre BLM property at the southwest corner of Horse Drive and Bradley Road, the developer shall construct and donate full off-site improvements to the school site. If the School District is unable to acquire property within the BLM site, the developer shall sell a 12 gross acre site, centrally located within Phases 3 or 4 of the development, to the School District for their cost. The developer shall construct full off-site improvements around the site.
34. The site plan shall be revised to comply with the R-PD (Residential Planned Development) Open Space requirements of the Zoning Code.
35. The site plan shall be revised to comply with the Parks, Open Space and Trails Plan as required by the Planning and Development Department. All trails shall be constructed concurrently with this development and maintained by a Homeowner's Association until such time as ownership of the trails is accepted by the City. All loop trails interior to the subdivision sites shall be maintained by the Homeowner's Association in perpetuity.
36. The applicant shall revise the Master Plan and Development Standards of this project as required by the Planning and Development Department.
37. Appropriate Reversionary Maps must be recorded prior to the issuance of any building or grading permits or the recordation of any Final Maps as required by the Department of Public Works.
38. Obtain City Council approval of a Vacation Application to vacate all public rights-of-way in conflict with this proposal adjacent to this site prior to the submittal of a Final Map as required by the Department of Public Works. Also, submit to the City a copy of a Clark County Vacation Application approved by the Clark County Commission to vacate all existing public right-of-ways within the County in conflict with this proposed site layout prior to the submittal of a Final Map. A Tentative Map proposing to incorporate the areas proposed to be vacated may be submitted prior to formal approval of the Vacation Applications by the City Council/County Commission; however, if the required Vacation cannot be approved for any reason; such Tentative Maps shall be rendered NULL AND VOID and a new Tentative Map showing such existing rights-of-way shall be submitted. The Vacation applications shall record prior to the recordation of a Final Map overlying any existing public rights-of-way on this site.

39. Dedicate appropriate public street rights-of-way including appropriate corner radii for all public streets proposed within this development as required by the Department of Public Works. Site-specific dedication requirements shall be imposed with subsequent development actions. Major street dedication requirements are subject to the recommendations of the approved Traffic Access Analysis Report and required amendments to the City's Master Plan of Streets and Highways proposed therein.
40. Obtain City Council approval to amend the City's Master Plan of Streets and Highways as recommended by the approved Traffic Access Analysis Report prior to the submittal of any Final Maps on this site as required by the Department of Public Works. A Tentative Map proposing to incorporate the public streets proposed to be amended may be submitted prior to formal approval of the amendment to the Master Plan of Streets and Highways by the City Council/County Commission; however, if the required amendment cannot be approved for any reason; such Tentative Maps shall be rendered NULL AND VOID and a new Tentative Map showing the appropriate rights-of-way shall be submitted. The proposed amendment to the Master Plan of Streets and Highways must be approved by the City Council prior to the recordation of a Final Map overlying any conflicting public rights-of-way on this site.
41. Construct half-street (or full-width, if applicable) improvements including appropriate overpaving (if legally able) on Jones Boulevard, Decatur Boulevard, Thom Boulevard, Whispering Sands Drive, Iron Mountain Road, Horse Drive, Brent Lane, Grand Teton Drive, and Racel Street adjacent to this site concurrent with development of this site as required by the Department of Public Works. **Rural street improvements may be allowed on Horse Drive and Bradley Road north of Grand Teton Drive subject to the approval of the Master Drainage Study and Master Traffic Access Analysis Report allowing such.** A phasing plan detailing the timing of the construction of all required improvements must be submitted to and approved by the Department of Public Works prior to the issuance of grading, building or off-site improvements or the recordation of any map dividing this project site, whichever may occur first; all required offsite improvements shall be constructed in accordance with such approved plan. Provide two lanes of paved, legal access to each phase of this project site from an existing paved public street prior to occupancy of any units within each phase. Where this site is required to construct public street improvements on two sides of a not-a-part piece or is needed for pavement continuity, widened paving shall be constructed adjacent to the not-a-part piece unless specifically allowed otherwise as determined by the City Planning Engineer. Additional paved access routes, including the development of temporary access road improvements on Decatur Boulevard and/or the upgrading of Jones Boulevard with additional traffic lanes may be required if recommended by the approved Traffic Access Analysis Report or if requested by the Traffic Engineer based on construction traffic patterns and/or the impact of this rezoning site's traffic on the surrounding neighborhood. Install all appurtenant

- underground facilities, if any, adjacent to this site needed for the future traffic signal systems concurrent with development of this site. All existing overpaving damaged or removed by this development shall be restored at its original location and to its original width concurrent with development of this site. The City reserves the right to impose requirements for phased development with subsequent "pod" development actions.
42. Coordinate the Extension and Oversizing to bring public sanitary sewer to this site to a location and along an alignment acceptable to the City Sanitary Planning Engineer. All required public sewer easements necessary to connect this site to the existing public sewer system shall be provided to the City prior to City approval of sewer construction plans, or the issuance of any off-site permits as required by the Department of Public Works. Additional public sanitary sewer lines may be required to be extended to this project site; such requirements shall be dictated by the City Planning Engineer; phased development and compliance with this condition is acceptable if more than one public sewer line is required.
 43. The interior residential public street cross sections are acceptable as proposed; pedestrian walkway easements shall be provided for all public sidewalks not located within public right-of-way as required by the Department of Public Works. Four foot sidewalks may be allowed for the interior residential public streets provided that no obstructions (i.e. light poles, fire hydrants or utility boxes) are located within that fourfoot area. Ten foot paved travel lanes may be allowed for the interior residential public streets subject to the approval of Fire Services and the Traffic Access Analysis Report.
 44. A Master Traffic Access Analysis Report must be submitted to and approved by the Department of Public Works prior to the issuance of any building or grading permits or the recordation of any Final Maps for this site, whichever may occur first. Comply with the recommendations of the approved Traffic Access Analysis Report prior to occupancy of the site. The Traffic Access Analysis Report shall: a) assess adequacy of street improvements leading to this site; b) assess the adequacy of intersections leading to this site; c) establish Safe Route to School corridors and the need for improvements along such corridors, and d) include a section addressing Standard Drawings #234.1 and #234.2 to determine additional right-of-way requirements adjacent to this site. All additional rights-of-way required by the above-referenced Standard Drawings shall be dedicated (and shall appear on all Tentative and Final Maps for this site) unless specifically noted as not being required in the approved Traffic Impact Analysis. Additionally this site shall be responsible for paying for the neighborhood traffic signal contributions amounting to \$400 per residential unit or \$1500 per acre, whichever is greater. The Master Access Analysis Report shall propose an implementation program for the construction for all on-site and off-site improvements, including a phasing plan identifying appropriate milestones (such as number of units built) that will trigger the timely construction thereof. In addition, the Traffic Access Analysis Report shall recommend changes to the City's Master Plan of Streets and Highways to correspond with the reduction in public street rights-of-way and possible

realignments (this report should specifically address the realignment of the City's proposed Decatur/Iron Mountain curve northwesterly to the Moccasin Road alignment proposed on this site. Phased compliance will be allowed if recommended by the approved Traffic Impact Analysis. No recommendation of the approved Traffic Impact Analysis, nor compliance therewith, shall be deemed to modify or eliminate any Condition of Approval imposed by the Planning Commission or the City Council on the development of this site.

45. A Master Drainage Plan and Technical Drainage Study must be submitted to and approved by the Department of Public Works prior to the issuance of any building or grading permits or the recordation of a Final Map, whichever may occur first, as required by the Department of Public Works. The required Drainage Plan/Study shall also address what off-project improvements, if any, may be necessary to mitigate any negative impacts to the properties and neighborhoods surrounding and downstream from this rezoning site based on this site's proposed discharge flow rates and discharge locations. Provide and improve all on-site drainage corridors and provide all on-site and off-project street, channel and pipe improvements as recommended in the approved Drainage Plan/Study.
46. Site-specific Drainage Plans and Traffic Access Analysis Reports may be required as each internal site develops as required by the Department of Public Works. The City reserves the right to impose additional site-specific conditions with future site development actions.
47. A Homeowner's Association shall be established to maintain all multi-use trails, perimeter walls, landscaping, and common areas created with this development. All landscaping shall be situated and maintained so as to not create sight visibility obstructions for vehicular traffic at all development access drives and abutting street intersections.
48. The City reserves the right to withhold building permit issuance for any and all lots within this rezoning site if adequate street improvements and/or adequate traffic capacity does not exist on the access corridors leading to this rezoning site as determined by the Director of the Department of Public Works. The City also reserves the right to withhold building permit issuance for any and all lots within this rezoning site if adequate street improvements and/or adequate traffic capacity does not exist on the issuance for any and all lots within this rezoning site if adequate intersection capacity does not exist at the Durango/US95 intersection as determined by the Director of the Department of Public Works. These rights shall be exercised based on the traffic capacity conditions that exist at the time of the developer's request for the issuance of each building permit. In the event of a dispute with the decision of the Director of the Department of Public Works, the developer may appeal directly to the City Council for final resolution.

Mr. Donald R. Cavan
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49. The final layout of each "pod" within this development shall be determined at the time of approval of individual Tentative Maps for each "pod" site.
50. A Resolution of Intent with a two-year time limitation.
51. All City Code requirements and design standards of all City departments must be satisfied.
52. A fully operational fire protection system, including fire apparatus roads, fire hydrants and water supply, shall be installed and shall be functioning prior to construction of any combustible structures.
53. Where new water mains are extended along streets and fire hydrants are not needed for protection of structures, hydrants shall be spaced at a maximum distance of 1,000 feet to provide for transportation hazards.
54. Fence heights shall be measured from the side of the fence with the least vertical exposure above the finished grade.

Sincerely,



BARBARA JO RONEMUS
City Clerk

By: Beverly K. Bridges
Chief Deputy City Clerk

/ac

cc: Planning and Development Dept.
Development Coordination-DPW
Dept. Of Fire Services
Land Development Services

Mr. George Holman, Esquire
2500 West Sahara Avenue, Suite 202
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