

No.	Bearing	Distance
L1	N 89°26'21" E	3.50'
L2	N 89°26'21" E	5.00'
L3	S 00°33'39" E	14.00'
L4	S 00°33'39" E	5.00'
L5	N 00°33'39" W	17.00'
L6	S 00°33'39" E	17.00'
L7	S 00°43'11" E	9.00'
L8	N 89°26'21" E	14.87'
L9	N 89°26'21" E	16.62'
L10	N 89°26'21" E	16.72'
L11	N 89°26'21" E	16.89'
L12	N 89°26'21" E	17.01'
L13	N 89°26'21" E	17.04'
L14	S 00°33'39" E	10.07'
L15	S 42°44'43" E	83.95'
L16	N 89°26'21" E	1.02'
L17	S 00°33'39" E	17.56'
L18	S 89°26'21" W	15.50'
L19	N 89°26'21" E	11.00'
L20	N 89°26'21" E	25.70'
L21	N 00°33'39" W	11.83'
L22	N 00°33'39" W	12.80'
L23	N 89°26'21" E	3.60'
L24	N 89°26'21" E	14.60'
L25	N 89°26'21" E	110.87'
L26	N 00°33'39" W	13.69'
L27	S 00°33'39" E	146.09'
L28	S 00°33'39" E	13.13'
L29	N 89°26'21" E	5.08'
L30	S 00°33'39" E	6.09'
L31	S 00°33'39" E	8.83'
L32	N 89°26'21" E	0.33'
L33	N 89°26'21" E	0.44'
L34	S 00°33'39" E	141.82'
L35	S 00°33'39" E	8.34'
L36	N 90°00'00" E	0.00'
L37	N 00°33'39" W	15.00'
L38	S 00°33'39" E	140.31'
L39	S 89°26'21" W	5.54'
L40	N 00°33'39" W	14.05'
L41	N 89°26'21" E	28.45'
L42	N 00°33'39" W	5.00'

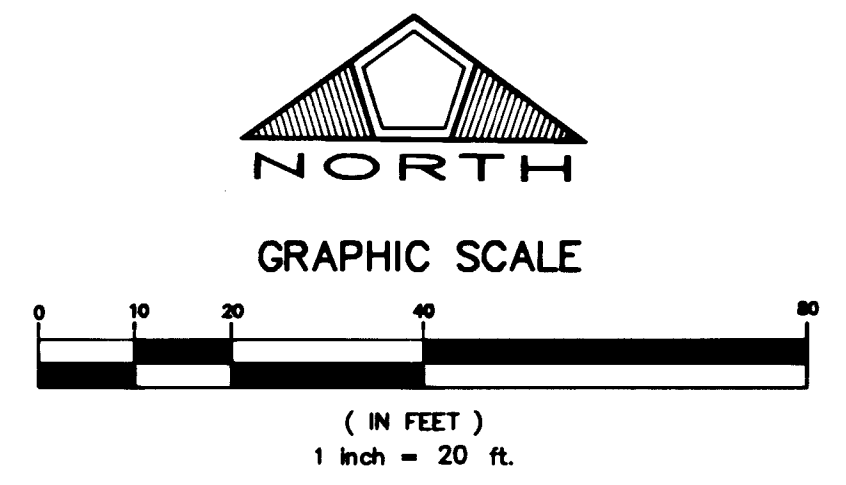
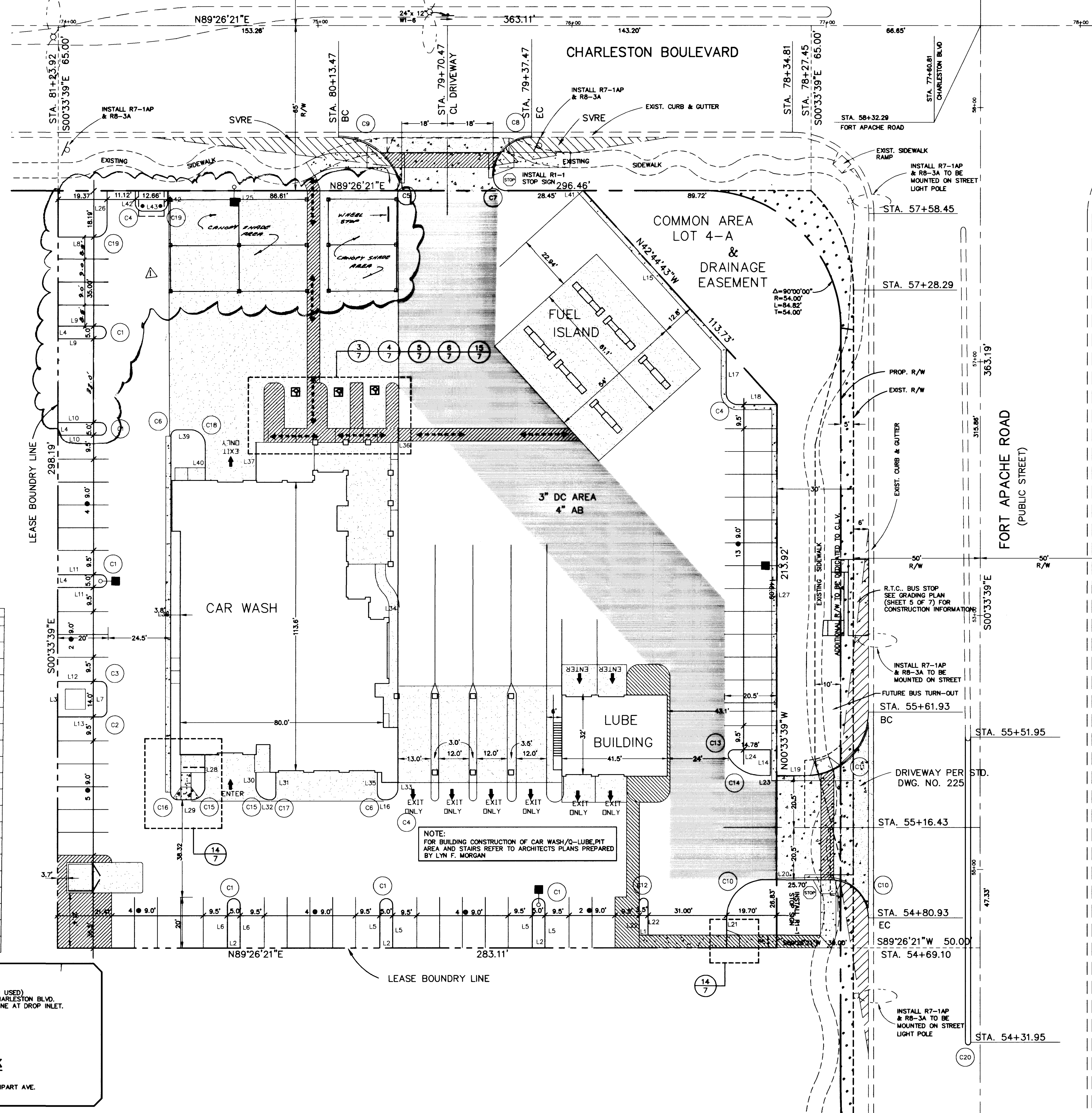
No.	Radius	Delta	Length	Tangent
C1	2.50'	180°00'00"	7.85'	0.00'
C2	2.50'	90°09'32"	3.93'	2.51'
C3	2.50'	89°50'28"	3.92'	2.49'
C4	4.50'	90°00'00"	7.07'	4.50'
C5	3.00'	108°34'00"	5.68'	4.17'
C6	2.50'	90°00'00"	3.93'	2.50'
C7	5.00'	84°15'39"	7.35'	4.52'
C8	15.00'	95°44'21"	25.06'	16.58'
C9	25.00'	71°26'00"	31.17'	17.98'
C10	15.00'	90°00'00"	23.56'	15.00'
C11	25.00'	90°00'00"	39.27'	25.00'
C12	1.75'	180°00'00"	5.50'	0.00'
C13	2.00'	122°13'07"	4.27'	3.62'
C14	15.00'	57°46'53"	15.13'	8.28'
C15	5.50'	81°21'15"	7.81'	4.73'
C16	3.00'	90°00'00"	4.71'	3.00'
C17	2.00'	90°00'00"	3.14'	2.00'
C18	5.90'	86°45'34"	8.93'	5.57'
C19	3.00'	90°00'00"	4.71'	3.00'
C20	2.00'	180°00'00"	3.20'	0.00'

BENCHMARK

NATIONAL GEODETIC VERTICAL DATUM OF 1929 (DATUM USED)
 QLV0031558W, RIVET & PLATE IN TOP OF CURB ON CHARLESTON BLVD.
 1690' WEST OF GRAND CANYON DRIVE, NORTH CURB LINE AT DROP INLET.
 CITY OF LAS VEGAS DATUM
 ELEVATION = 2865.191
 (FOR CONVERSION TO 1988 DATUM, ADD 2.46')

TEMPORARY BENCHMARK

2" BRASS CAP STAMPED "PLS 5094", SET IN ASPHALT
 AT THE INTERSECTION OF CHARLESTON BLVD. AND RAMPART AVE.
 ELEVATION = 2744.34 (NGVD 29)



NOTE:
SEE SHEET 7 OF 7 FOR
A.D.A. STANDARD DETAILS

Call before you Dig.
1-800-227-2600
UNDERGROUND SERVICE (USA)

Call before you OVERHEAD
1-702-593-6111
NEVADA POWER TRANSMISSION
DIVISION

BASIS OF BEARING
 NORTH 89°26'21" EAST, BEING THE BEARING OF THE SOUTH LINE OF THE SOUTHWEST QUARTER (SW 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SECTION 32, TOWNSHIP 20 SOUTH, RANGE 60 EAST, MOUNT DIABLO MERIDIAN AS SHOWN ON THAT CERTAIN PLAT ON FILE IN THE OFFICE OF THE CLARK COUNTY RECORDER IN BOOK 45, PAGE 83, OF PLATS

- ENGINEER'S NOTES**
- 1) ALL BOUNDARY AND EASEMENT INFORMATION TO BE VERIFIED PRIOR TO CONSTRUCTION.
 - 2) VERIFY TRASH ENCLOSURE DETAILS WITH ARCHITECT'S PLANS.
 - 3) ALL DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
 - 4) DEVELOPER MAY SUBSTITUTE EXTENDED SIDEWALK INSTEAD OF "A" CURB WHERE APPLICABLE.
 - 5) SEE ARCHITECT'S PLAN FOR BUILDING DIMENSIONS AND FOR ADDITIONAL HORIZONTAL CONTROL INFORMATION.
 - 6) CONTRACTOR RESPONSIBLE FOR ADHERING TO ALL ADA PROVISIONS.
 - 7) NOTIFY ENGINEER FOR ANY DISCREPANCIES IMMEDIATELY.

LEGEND:

- REFERENCE AREA
- DETAIL SHEET REFERENCE
- A.D.A. ACCESSIBLE ROUTE
- A.D.A. ACCESSIBLE ROUTE
- HEAVY DUTY PAVEMENT
- PROPOSED ADDITIONAL R/W
- CONCRETE PAVEMENT

- GENERAL NOTES**
- NOTE:
SEE ARCHITECTS AND LANDSCAPE DRAWINGS FOR ADDITIONAL INFORMATION ON GRADES AND HARDSCAPE DESIGN.
- HANDICAP PARKING SPACES:**
1. SINGLE SPACE - 14' WIDE MINIMUM; WITH 9' WIDE PARKING SPACE AND 5' WIDE LOADING AREA
 2. DOUBLE SPACE - 23' WIDE MINIMUM; WITH TWO 9' WIDE PARKING SPACES AND 5' WIDE CENTER LOADING AREA
 3. SLOPE - SLOPE OF SPACES SHALL NOT EXCEED 1:50
 4. PARKING SPACES ARE CONSIDERED PART OF THE ACCESSIBLE ROUTE OF TRAVEL. EACH SPACE SHALL HAVE A SIGN SHOWING THE SYMBOL FOR ACCESSIBILITY LOCATED SO THAT A PARKED VEHICLE DOES NOT OBSCURE IT
- ACCESSIBLE ROUTE:**
- WIDTH - MINIMUM WIDTH OF AN ACCESSIBLE ROUTE SHALL BE 60"
- SLOPE - ACCESSIBLE ROUTE SHALL BE A MAXIMUM SLOPE OF 1:20 WITH A 1:50 MAXIMUM CROSS SLOPE
- NOTE: - AN ACCESSIBLE ROUTE WITH A SLOPE GREATER THAN 1:20 IS CONSIDERED A RAMP
- CURB RAMPS**
- LOCATION - WHENEVER AN ACCESSIBLE PATH OF TRAVEL CROSSES A RAISED CURB
- SLOPE - MAXIMUM SLOPE OF ADJOINING SURFACES IS 1:20. SLOPE OF CURB RAMP IS A MAXIMUM OF 1:12
- SIDES - WHERE PEDESTRIANS WALK ACROSS RAMP OR IF NOT PROTECTED BY HANDRAILS OR GUARDRAILS, FLARED SIDES WITH A MAXIMUM SLOPE OF 1:10 ARE NECESSARY. WHERE THERE IS NO PEDESTRIAN CROSS TRAFFIC, RETURNED CURBS MAY BE USED
- WIDTH - 36" MINIMUM WIDE, EXCLUSIVE OF FLARED SIDES

NO.	DESCRIPTION	DATE	APP'D
1	ADD CANYON SHADE AREA & RELOCATE DRIVEWAY ARE CITY REQUEST	12/18/24	MMH

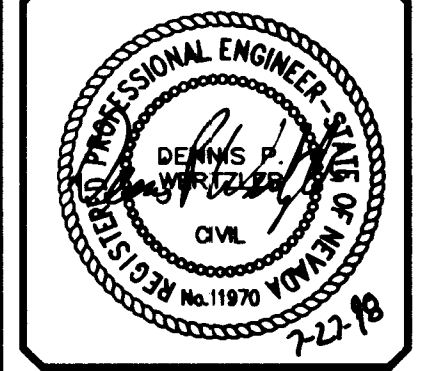
MR. FRED SMITH
 1901 SKI SLOPE CIRCLE
 LAS VEGAS, NEVADA, 89117
 (702)255-3446

PENTACORE
 CIVIL ENGINEERING, LAND SURVEYING & PLANNING
 CONSULTING ENGINEERS
 8703 WEST CHARLESTON BOULEVARD
 LAS VEGAS, NEVADA 89102 (702)256-0115

HORIZONTAL CONTROL/A.D.A. ACCESS/ AND SURFACING PLAN

PECCOLE CAR WASH

DATE: MAY 26, 1998	SPITZE
DESIGNED BY: [Signature]	CHECKED BY: [Signature]
ADVA CHECKED: [Signature]	JOB NO.: 050.0222
SCALE: 1" = 20'	



0:\0050\0222\H-COMTROL PLAN.DWG Mon Jul 27 11:50:17 1998 DunWayne A

H-COMTROL / ADA ACCESS /

