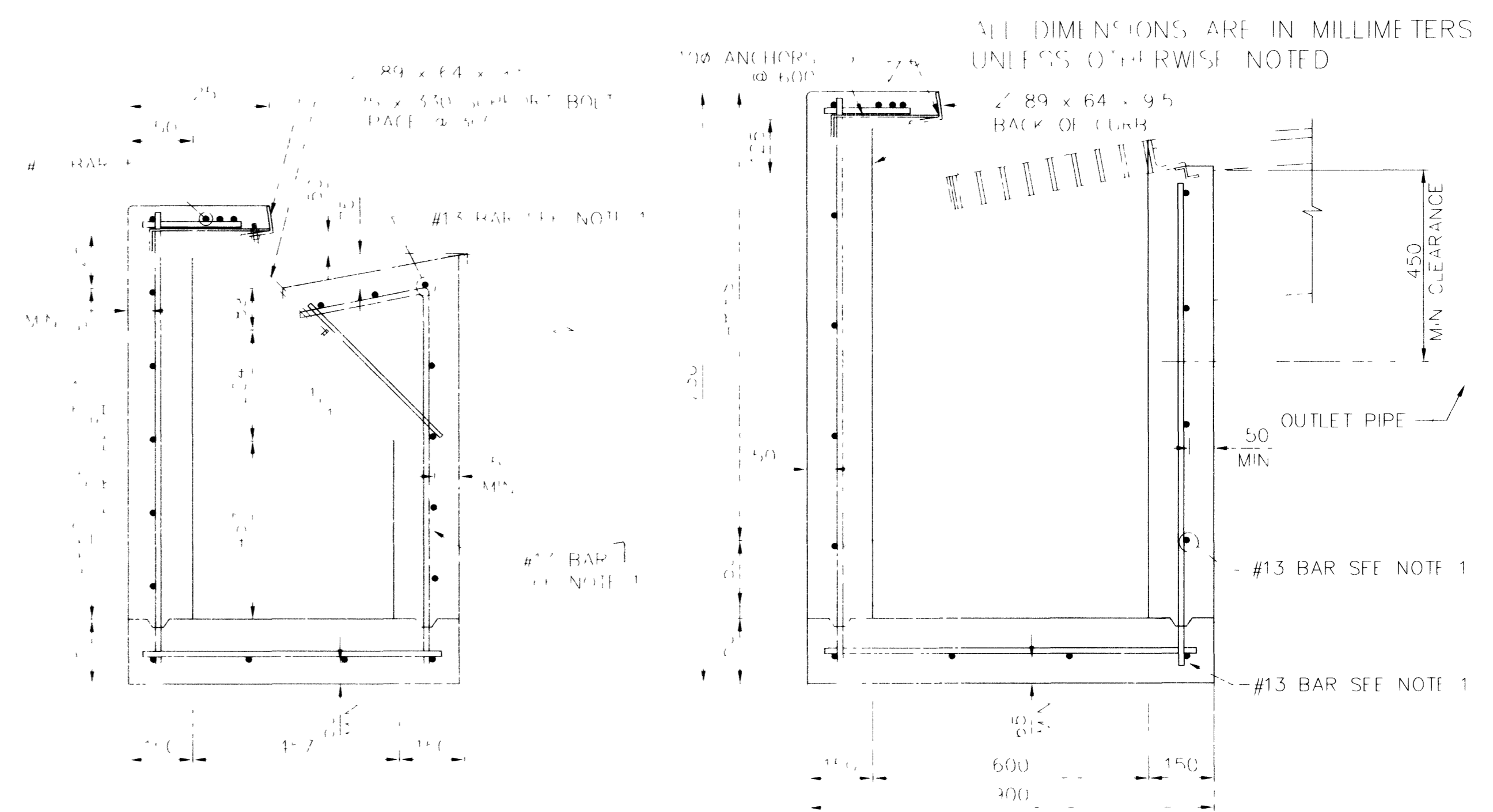


PLAN
SOME CONNECTION



SECTION "C-C" SECTION "D-D"

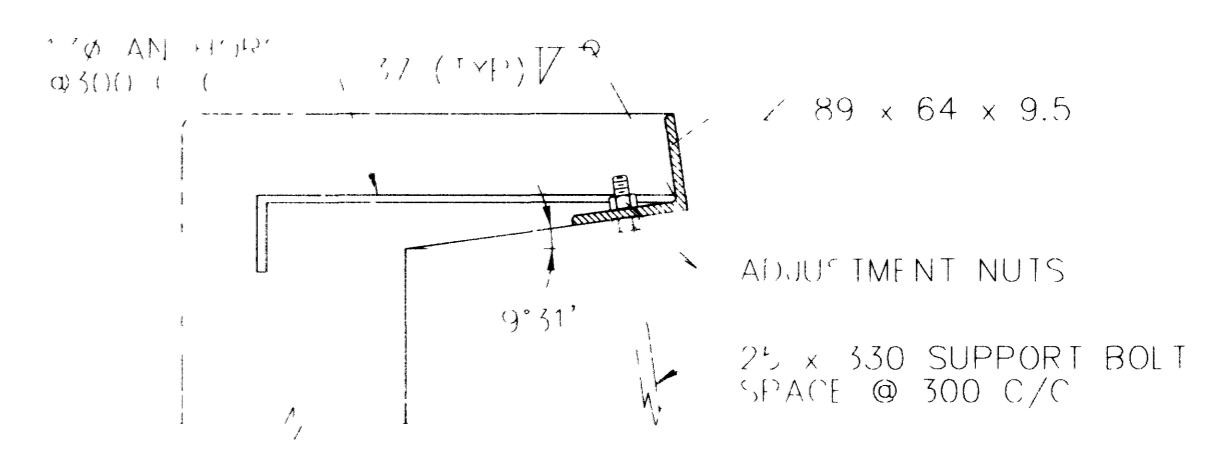
DROP INLET TYPE "D" MODIFIED

ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE NOTED



SECTION "E-E"

- 1. ALL CONCRETE SHALL BE #5 BARS EXCEPT AS NOTED, WITH MAX. SPACING AT 500mm CENTER TO CENTER, WITH MIN. 25mm CLEARANCE FROM ALL SURFACES.
- 2. ALL REINFORCING SHALL BE GALVANIZED AFTER FABRICATION.
- 3. ALL JOINTS SHALL BE GALVANIZED AFTER FABRICATION.
- 4. ALL JOINTS SHALL BE GALVANIZED AFTER FABRICATION.
- 5. ALL JOINTS SHALL BE GALVANIZED AFTER FABRICATION.
- 6. ALL JOINTS SHALL BE GALVANIZED AFTER FABRICATION.
- 7. ALL JOINTS SHALL BE GALVANIZED AFTER FABRICATION.
- 8. ALL JOINTS SHALL BE GALVANIZED AFTER FABRICATION.
- 9. ALL JOINTS SHALL BE GALVANIZED AFTER FABRICATION.
- 10. ALL JOINTS SHALL BE GALVANIZED AFTER FABRICATION.

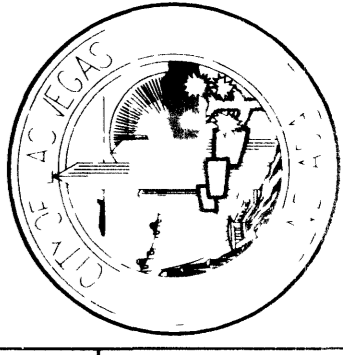


STEEL PLATE AND SUPPORT BOLT DETAIL

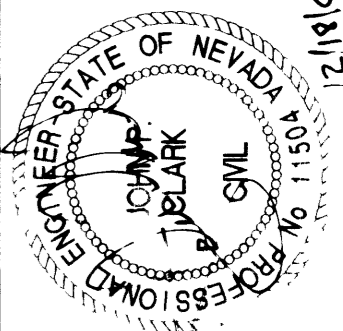


EXISTING 600mm RCP

SCALE (H) N/A	SCALE (V) N/A	PROJECT # 1504
F.D. BK. # N/A	DATE: DECEMBER 17, 1999	DESIGN BY



DEPARTMENT OF PUBLIC WORKS
I-15 FREEWAY CHANNEL
DROP INLET DETAILS @ RICHARD COURT



DRAWING No. S-7C